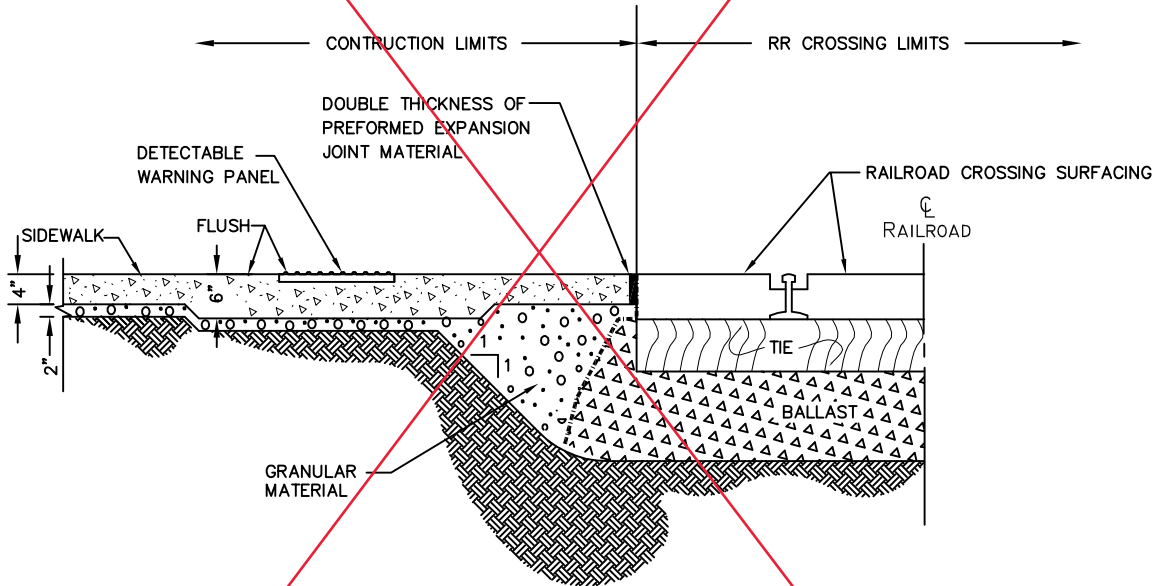
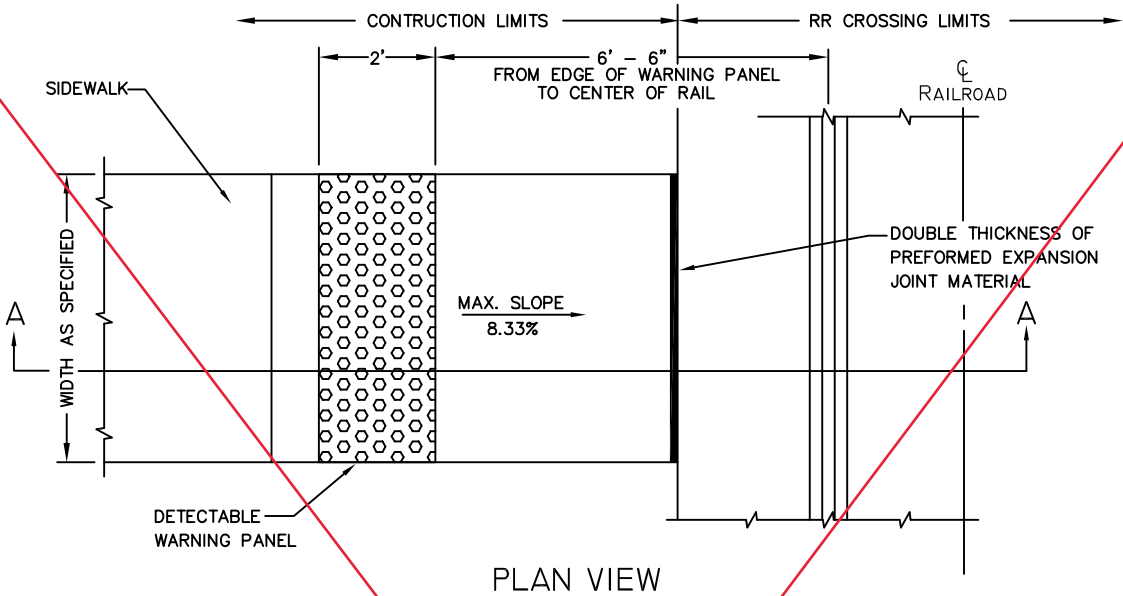


This plate will be replaced by plate 651.03



GENERAL NOTES:

SECTION A-A

THE MAXIMUM ALLOWABLE CROSS SLOPE OF THE SIDEWALK IS 2%.

THE CONTRACTOR MUST HAVE AN ELECTRONIC LEVEL ON SITE TO ENSURE THE SPECIFIED SLOPES ARE MAINTAINED.

IF CURB AND GUTTER IS ADJACENT TO THE SIDEWALK, THEN A CURB TRANSITION SHALL BE USED WITH A MAXIMUM LONGITUDINAL SLOPE OF 8.33%. THE SURFACE OF THE CURBSIDE SIDEWALK SHALL MATCH THE SLOPE OF THE CURB TRANSITION.

BALLAST MATERIAL SHALL NOT BE DISTURBED DURING CONSTRUCTION WORK ADJACENT TO THE RAILROAD CROSSING UNLESS THE ADJACENT WORK INVOLVES RECONSTRUCTION OR MAINTENANCE OF THE RAILROAD CROSSING.

CARE SHALL BE TAKEN TO ENSURE THE SURFACE OF THE DETECTABLE WARNING PANEL IS CLEAN AND FREE OF CURING COMPOUND AND CONCRETE RESIDUE.

THE DETECTABLE WARNING PANEL SHALL BE THE WIDTH OF THE SIDEWALK AND BRICK RED IN COLOR.

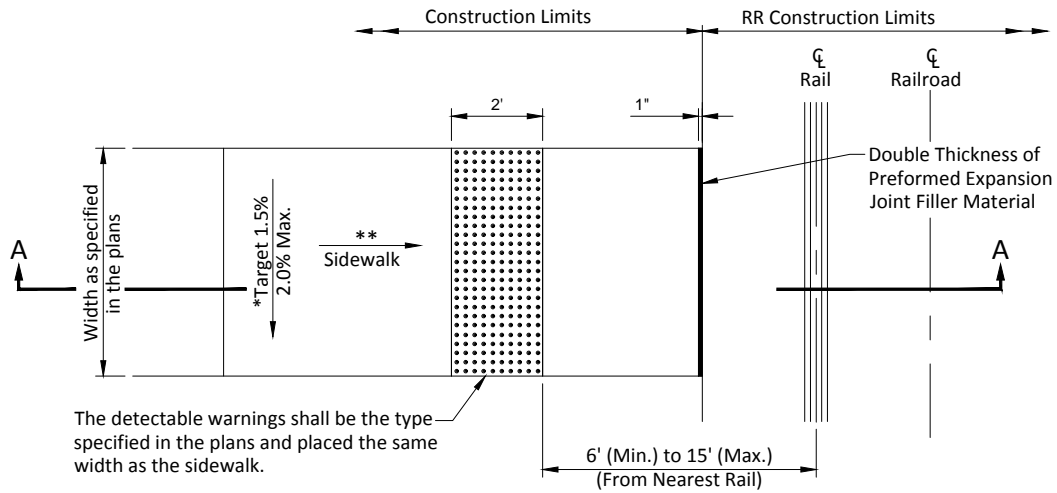
ISSUED: JANUARY 2005

SPECIFICATION
REFERENCE
NO.
651



CITY OF SIOUX FALLS
ENGINEERING DIVISION
SIDEWALK ADJACENT TO
RAILROAD CROSSING

PLATE
NUMBER
651.03

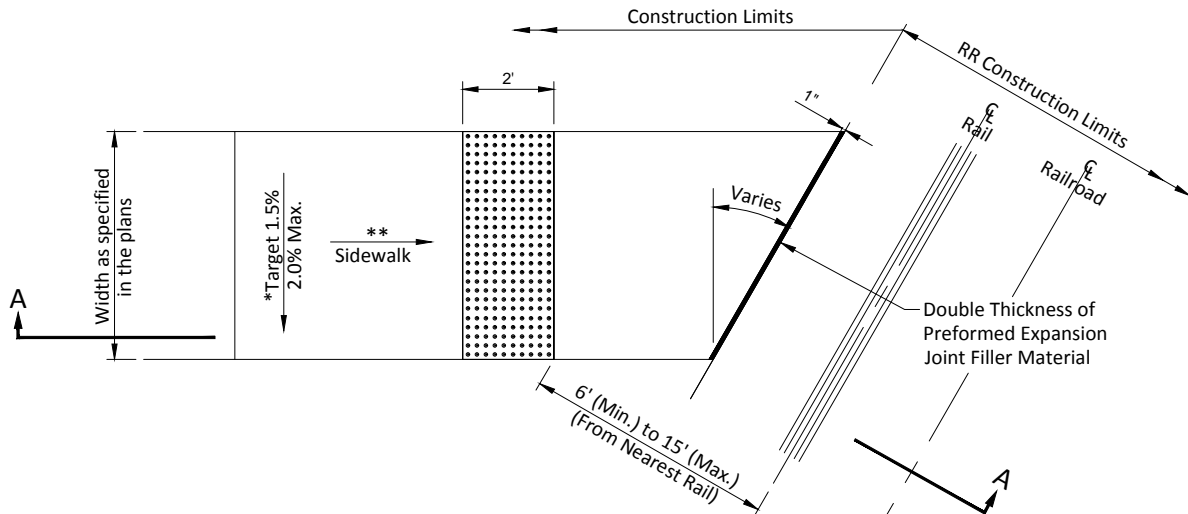


PLAN VIEW
(Railroad Crossing Not Skewed)

*The cross slope of the sidewalk shall not be steeper than 2%. Plans are designed using a 1.5% cross slope unless stated otherwise in the plans.

**If the sidewalk is curbside, then the surface of the curbside sidewalk shall match the slope of the curb transition. The longitudinal slope of the sidewalk and curb transition, where the sidewalk transitions to the railroad crossing elevation, is designed at 4.5% and shall not be steeper than 5% unless stated otherwise in the plans.

**If there is a boulevard sidewalk, then the curb and gutter transition shall match the longitudinal slope of the sidewalk. The longitudinal slope of the sidewalk, where the sidewalk transitions to the railroad crossing elevation, is designed at 4.5% and shall not be steeper than 5% unless stated otherwise in the plans.



PLAN VIEW
(Railroad Crossing Skewed)

Issued: October 2016

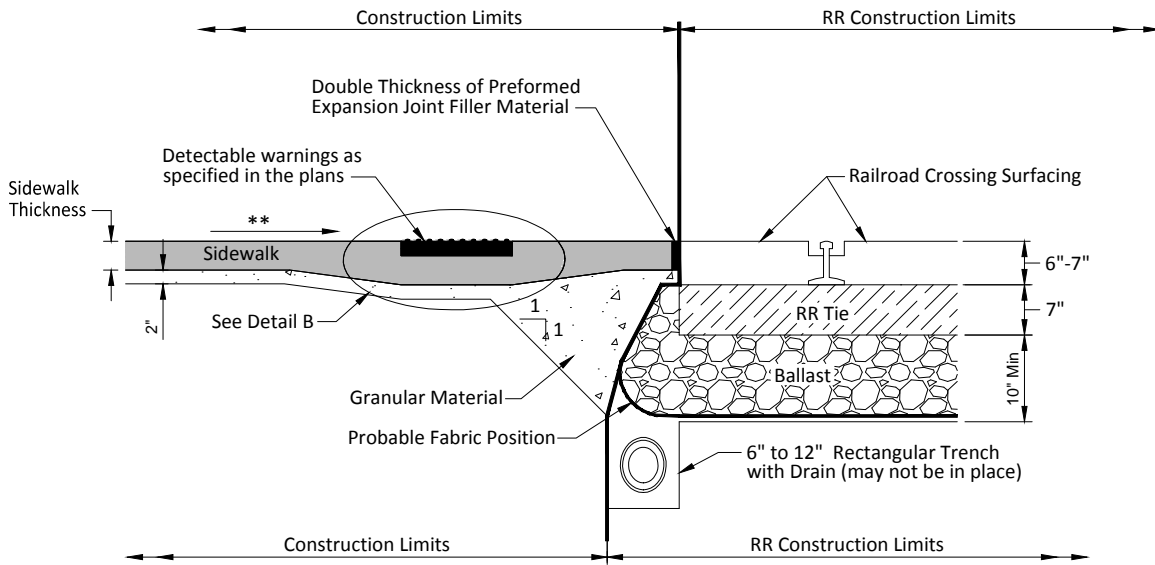
SPECIFICATION
REFERENCE
NO. 651



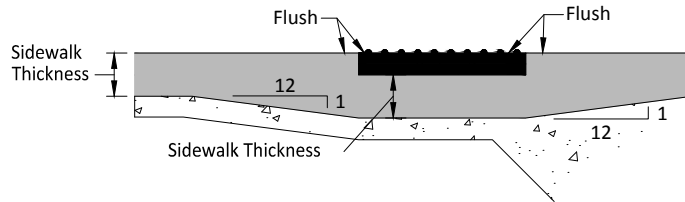
CITY OF SIOUX FALLS
ENGINEERING DIVISION
**SIDEWALK AND DETECTABLE WARNING
ADJACENT TO RAILROAD CROSSING**

PLATE NUMBER
651.03

Sheet No. 1 of 2



SECTION A-A



DETAIL B

GENERAL NOTES:

Ballast material shall not be disturbed during construction work adjacent to the railroad crossing unless the adjacent work involves reconstruction or maintenance of the railroad crossing.

The sidewalk shall be placed at the location stated in the plans.

Care shall be taken to ensure that the surface of the detectable warnings are clean and maintains a uniform color.

If curb and gutter is required adjacent to the railroad crossing, the curb transition shall be measured and paid for at the contract unit price per foot for the corresponding curb and gutter bid item.

The detectable warnings shall be measured to the nearest square foot. All costs for furnishing and installing the detectable warnings including labor, equipment, and materials, including adhesive, necessary sealant or grout, and necessary grinding shall be paid for at the contract unit price per square foot for Detectable Warnings.

The square foot area of the detectable warnings shall be included in the measured and paid for quantity of sidewalk.

Issued: October 2016

SPECIFICATION
REFERENCE
NO. 651



CITY OF SIOUX FALLS
ENGINEERING DIVISION
**SIDWALK AND DETECTABLE WARNING
ADJACENT TO RAILROAD CROSSING**

PLATE NUMBER
651.03

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